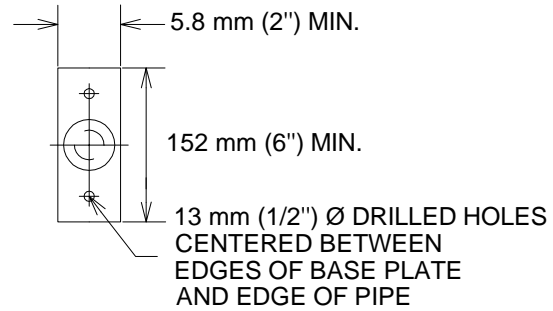
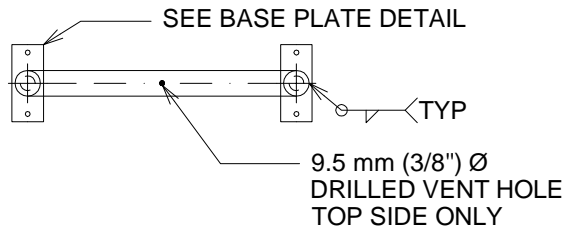


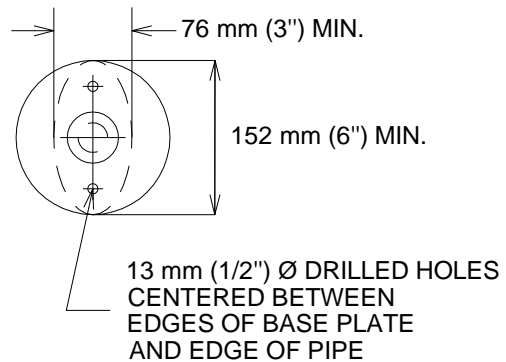
FRONT VIEW



BASE PLATE DETAIL



TOP VIEW



ALT BASE PLATE DETAIL
(CIRCULAR OR OVAL PLATE)

GENERAL NOTES:

1. RACK INSTALLATION METHOD SHALL COMPLY WITH CITY STANDARD DETAIL 710S-3, 710S-4, OR 710S-5.
2. RACK PLACEMENT SHALL COMPLY WITH APPLICABLE CITY STANDARD DETAILS 710S-6A, 710S-6B, OR 710S-6C AND CITY OF AUSTIN CODE SECTION 25-6-477
3. BASE PLATES TO BE 6.35 mm (1/4") PLATES, ASTM A-36 1010-1018 LOW CARBON PRIME STEEL

CITY OF AUSTIN
DEPARTMENT OF PUBLIC WORKS

CLASS III STYLE BICYCLE PARKING

RECORD COPY SIGNED
BY _____

XX/XX/XX

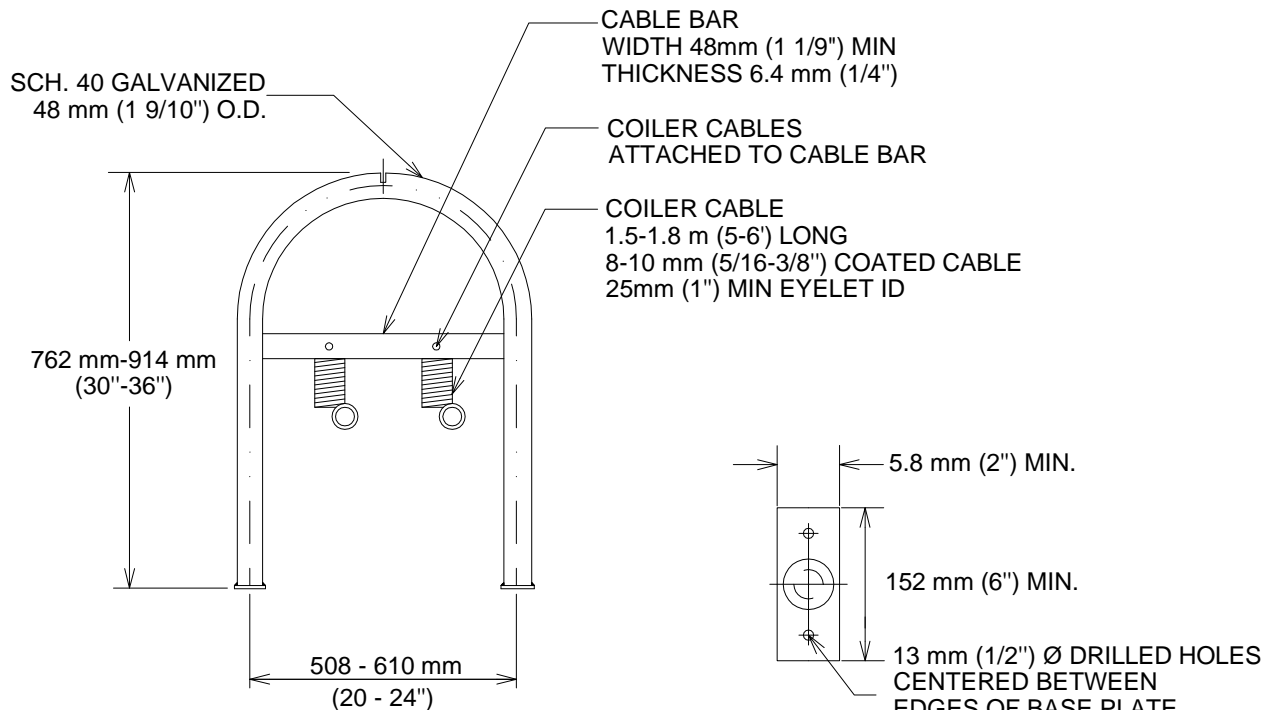
ADOPTED

THE ARCHITECT/ENGINEER ASSUMES
RESPONSIBILITY FOR APPROPRIATE USE
OF THIS STANDARD.

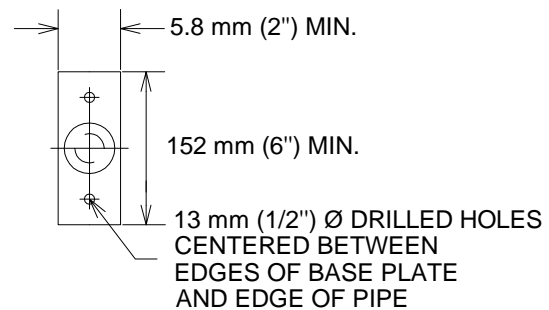
STANDARD NO.

710S-1

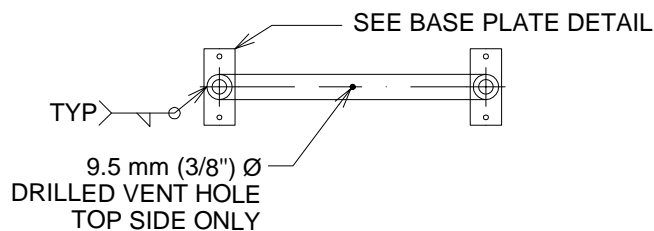
1 OF 1



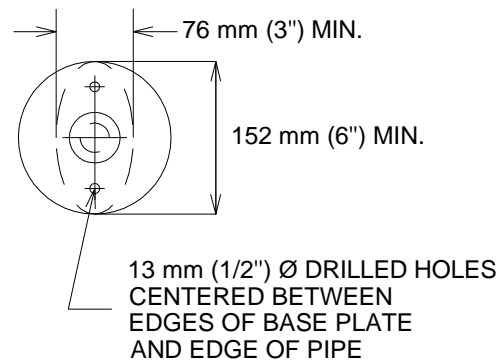
FRONT VIEW



BASE PLATE DETAIL



TOP VIEW



ALT BASE PLATE DETAIL
(CIRCULAR OR OVAL PLATE)

GENERAL NOTES:

1. RACK INSTALLATION METHOD SHALL COMPLY WITH CITY STANDARD DETAIL 710S-3, 710S-4, OR 710S-5.
2. RACK PLACEMENT SHALL COMPLY WITH APPLICABLE CITY STANDARD DETAILS 710S-6A, 710S-6B, OR 710S-6C AND CITY OF AUSTIN CODE SECTION 25-6-477
3. BASE PLATES TO BE 6.35 mm (1/4") PLATES, ASTM A-36 1010-1018 LOW CARBON PRIME STEEL

CITY OF AUSTIN
DEPARTMENT OF PUBLIC WORKS

CLASS II STYLE BICYCLE PARKING

RECORD COPY SIGNED
BY

XX/XX/XX

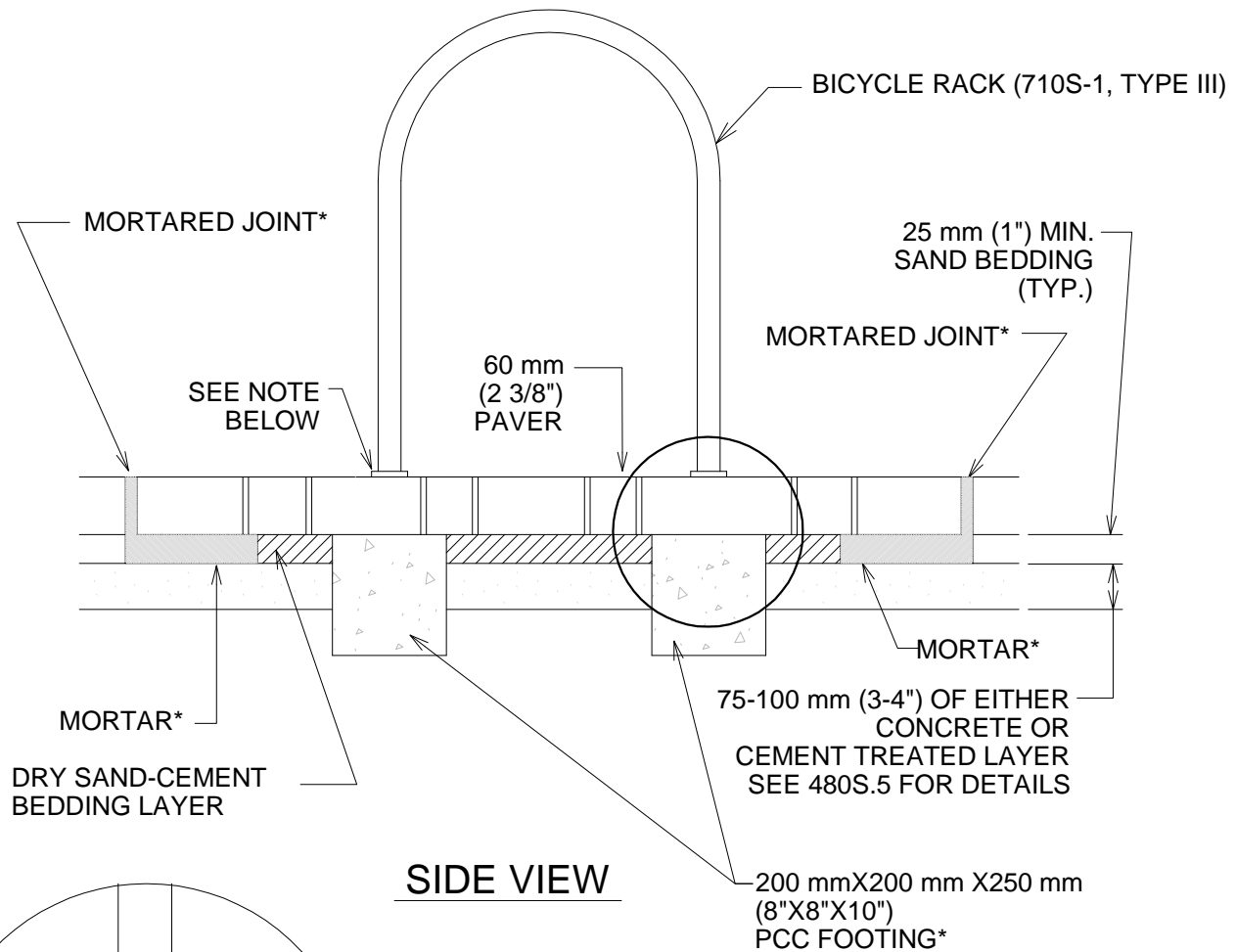
ADOPTED

THE ARCHITECT/ENGINEER ASSUMES
RESPONSIBILITY FOR APPROPRIATE USE
OF THIS STANDARD.

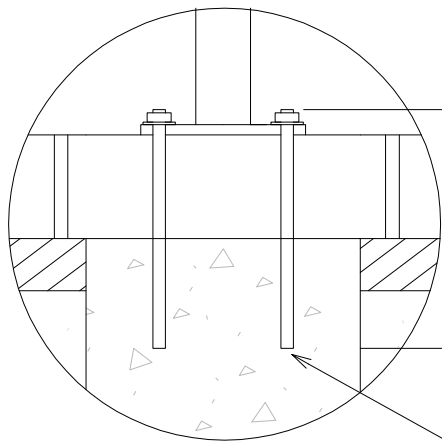
STANDARD NO.

710S-2

1 OF 1



SIDE VIEW



MOUNTING BOLT DETAIL

10 mm Ø X 150 mm (3/8"Ø X 6")
ANCHOR BOLTS EPOXIED IN PLACE.
SEE NOTES 7-10 ON SHEET 2 OF 2.

GENERAL NOTES:

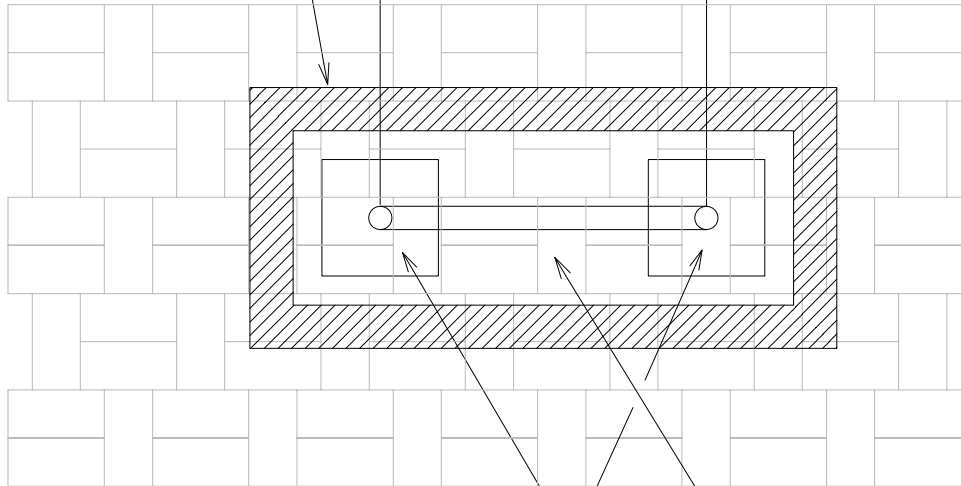
1. RACKS SHALL COMPLY WITH CITY STANDARD DETAIL 710S-1, TYPE III RACK FOR 1 TO 2 BIKES.
2. RACK PLACEMENT SHALL COMPLY WITH APPLICABLE CITY STANDARD DETAILS 710S-6A, 710S-6B, OR 710S-6C AND IN COMPLIANCE WITH SECTION 25-6-477 OF THE AUSTIN CITY CODE OR ITS SUCCESSOR.
3. SAW CUT PAVER TO MATCH PAVER CONFIGURATION.

*IF NECESSARY, SEE CONSTRUCTION SEQUENCE ON PAGE 2 FOR DETAILS

CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS		BICYCLE RACK INSTALLATION IN CONCRETE PAVER SIDEWALK-ALTERNATE 1	
RECORD COPY SIGNED BY	XX/XX/XX	THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	STANDARD NO. 710S-3 1 OF 2
	ADOPTED		

PAVERS WITH MORTAR*
BEDDING LAYER - FIRST FULL
UNITS AWAY FROM FOOTINGS

For Dimensions
See Detail 710 S-1



200 mmX200 mm X250 mm
(8\"X8\"X10\") PCC FOOTING*

PAVERS WITH DRY SAND-
CEMENT MIXTURE

TOP VIEW

CONSTRUCTION SEQUENCE:

1. PLACE BICYCLE RACK ON PAVERS AND MARK LOCATIONS OF BOLTHOLES AND REMOVE BICYCLE RACK.
2. DRILL THE HOLE LOCATIONS. IF CONCRETE IS HIT BELOW THE SAND CEMENT LAYER, ABOUT 100MM (4") DOWN THEN SKIP TO TO STEP 10. IF CONCRETE IS NOT PRESENT BELOW PAVERS A FOOTING PROCEED TO STEP 3 AS FOOTINGS WILL HAVE TO BE INSTALLED.
- **3. IDENTIFY LOCATION OF FOOTING.
- **4. MARK AND REMOVE EXISTING PAVERS THAT ARE THE FIRST FULL UNITS AWAY FROM FOOTING LOCATIONS, PLACE MORTAR BEDDING LAYER, MORTAR THE JOINT AND REPLACE/COMPACT THE "MARKED" PAVERS IN APPROPRIATE LOCATIONS.
- **5. MARK AND REMOVE EXISTING PAVERS FROM LOCATION ABOVE FOOTING LOCATIONS.
6. EXCAVATE FOR FOOTINGS AND PLACE CLASS "A" PCC CONCRETE.
7. PLACE DRY SAND-CEMENT BEDDING LAYER, REPLACE THE "MARKED" PAVERS IN APPROPRIATE POSITIONS AND COMPACT THE PAVERS IN PLACE.
8. PLACE BICYCLE RACK ON PAVERS AT APPROPRIATE LOCATIONS AND RE-MARK BOLT HOLES.
9. DRILL BOLT HOLES THROUGH THE PAVERS INTO THE PCC FOOTINGS.
10. INSTALL ANCHOR BOLTS AND EPOXY THEM IN PLACE.
11. INSTALL BICYCLE RACK AND BOLT IN PLACE.
12. TO PREVENT THEFT OF BICYCLE RACK OR BIKES, EXPOSED BOLTS MUST BE DEFORMED AND NUTS RE-TIGHTENED TO PREVENT THEM FROM BEING EASILY UNTHREADED. NUTS SHOULD BE TESTED TO ENSURE THAT THEY CANNOT BE EASILY REMOVED AFTER DEFORMATION.

*IF NECESSARY, SEE CONSTRUCTION SEQUENCE FOR DETAILS

** THESE STEPS ARE REQUIRED FOR EXISTING PAVR SIDEWALKS TO MAINTAIN STRUCTURE AND STABILITY OF ADJOINING PAVERS.

DRY SAND-CEMENT BEDDING PLACEMENT:

1. MATERIAL COMPOSED OF ONE PART CEMENT AND 3 PARTS SAND.
2. THE DRY MIXTURE SHALL BE LIGHTLY WETTED PRIOR TO PLACEMENT OF PAVERS.
3. AFTER COMPACTION OF PAVERS, JOINTS SHALL BE FILLED WITH DRY SAND-CEMENT.
4. THE COMPLETED JOINTS SHALL BE FOGGED LIGHTLY WITH WATER.

CITY OF AUSTIN
DEPARTMENT OF PUBLIC WORKS

BICYCLE RACK INSTALLATION IN
CONCRETE PAVR SIDEWALK-ALTERNATE 1

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BY _____

XX/XX/XX

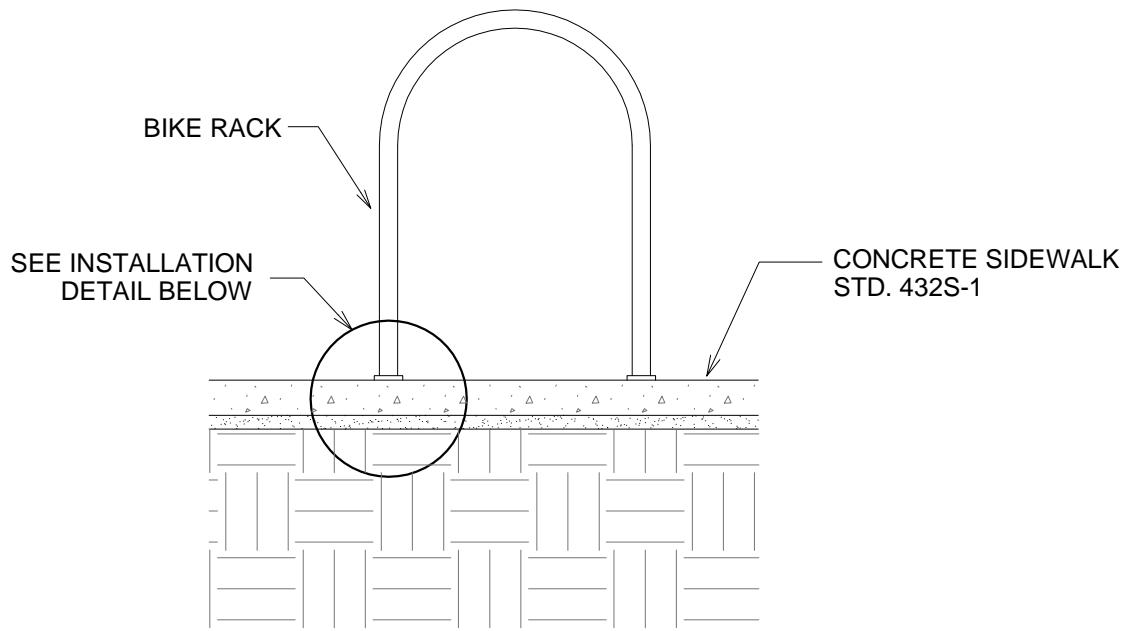
ADOPTED

THE ARCHITECT/ENGINEER ASSUMES
RESPONSIBILITY FOR APPROPRIATE USE
OF THIS STANDARD.

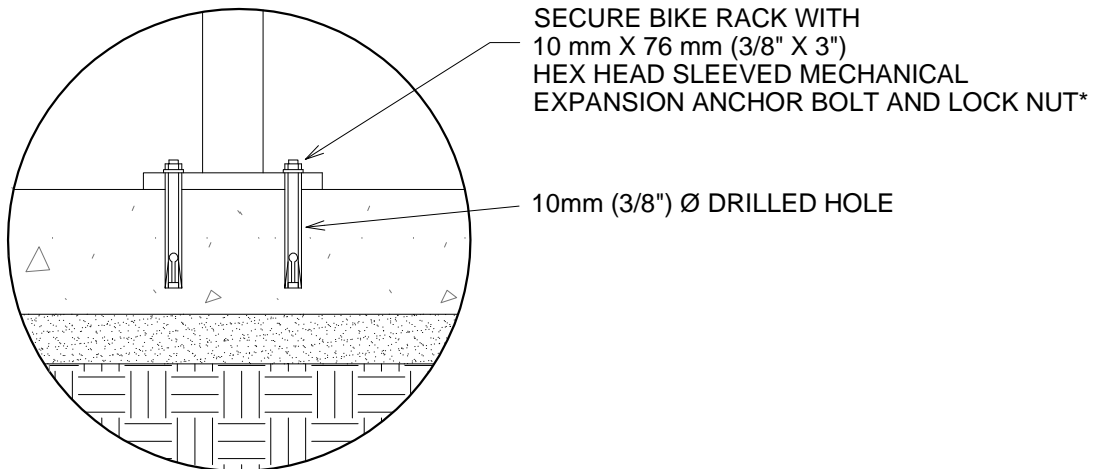
STANDARD NO.

710S-3

2 OF 2



BASIC INSTALLATION



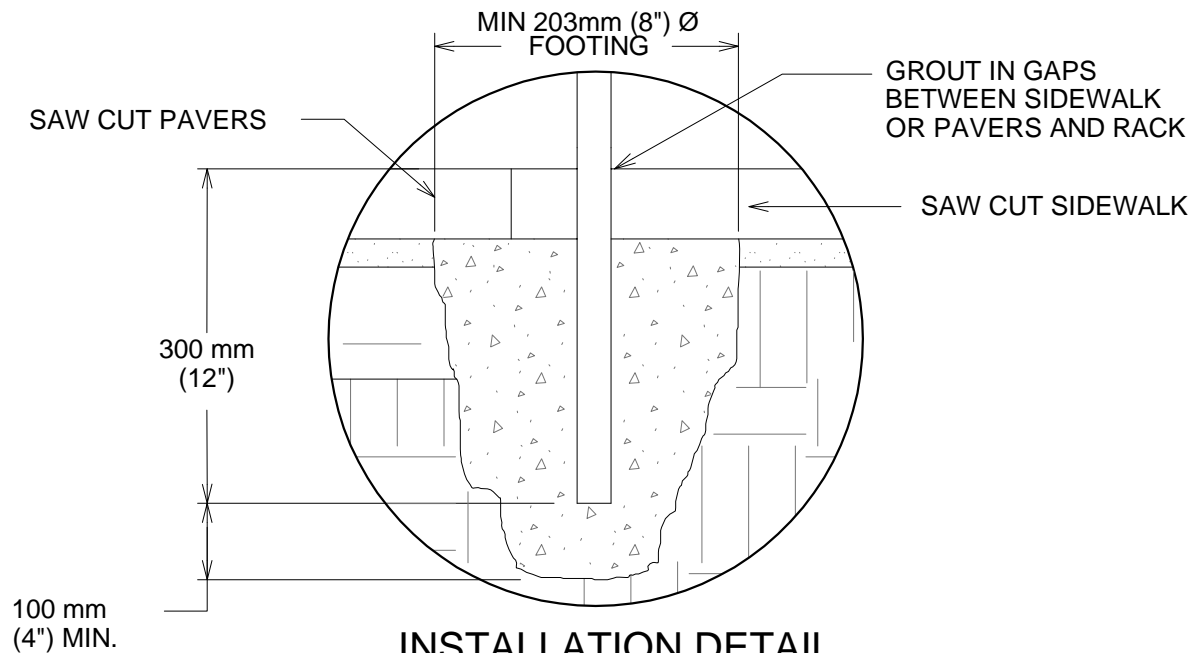
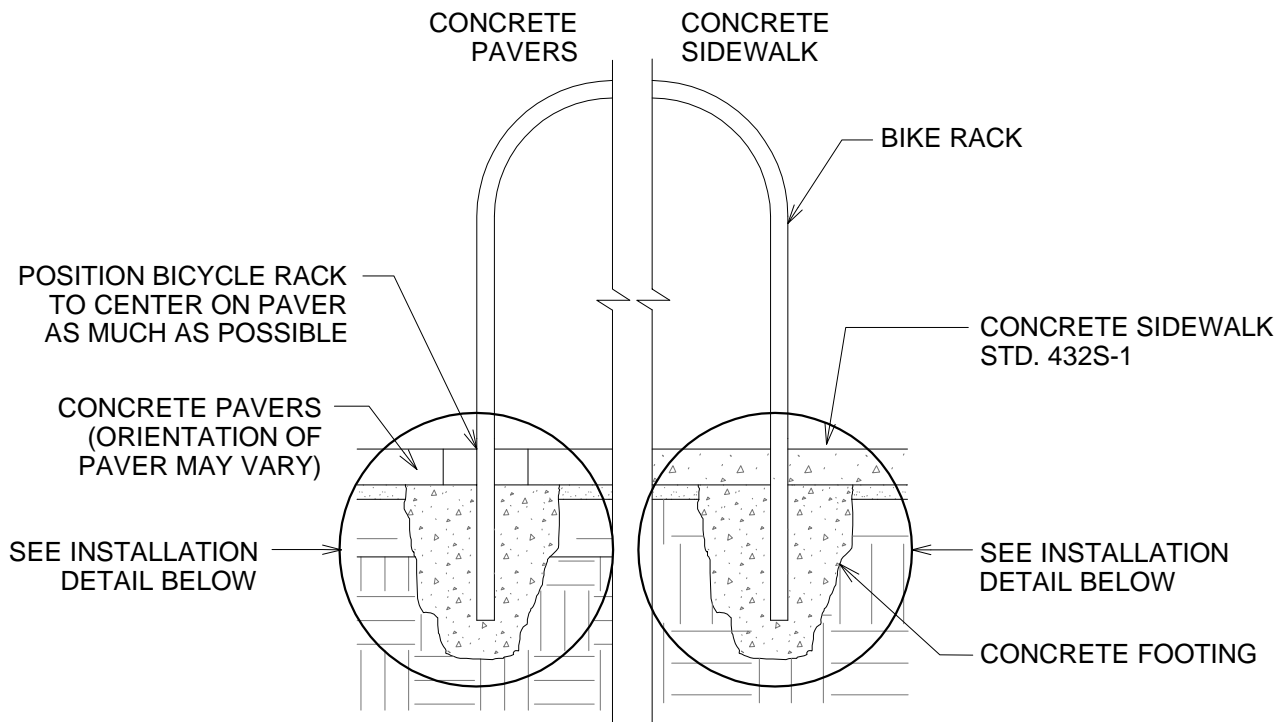
INSTALLATION DETAIL

GENERAL NOTES:

1. RACKS SHALL COMPLY WITH CITY STANDARD DETAIL 710S-1, TYPE III RACK FOR 1 TO 2 BIKES.
2. RACK PLACEMENT SHALL COMPLY WITH APPLICABLE CITY STANDARD DETAILS 710S-6A, 710S-6B, OR 710S-6C AND IN COMPLIANCE WITH SECTION 25-6-477 OF THE AUSTIN CITY CODE OR ITS SUCCESSOR.

*TO PREVENT THEFT OF BICYCLE RACK OR BIKES, EXPOSED BOLTS MUST BE DEFORMED AND NUTS RE-TIGHTENED TO PREVENT THEM FROM BEING EASILY UNTHREADED. NUTS SHOULD BE TESTED TO ENSURE THAT THEY CANNOT BE EASILY REMOVED AFTER DEFORMATION.

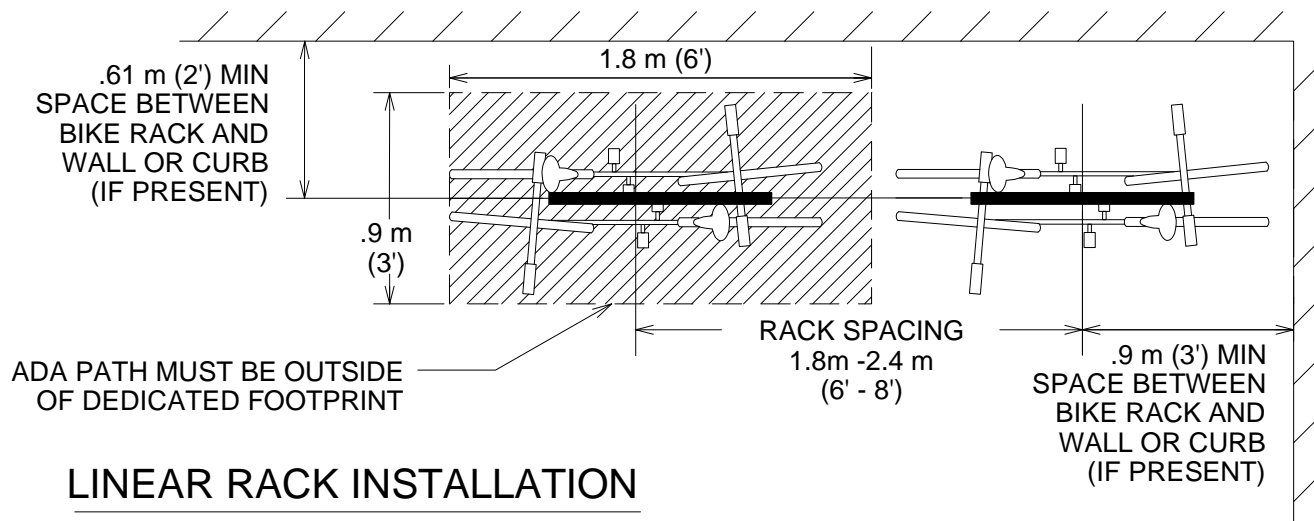
CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS		BICYCLE RACK INSTALLATION IN CONCRETE SIDEWALK-ALTERNATE 1	
RECORD COPY SIGNED BY _____	XX/XX/XX ADOPTED	THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	STANDARD NO. 710S-4 1 OF 1



GENERAL NOTES:

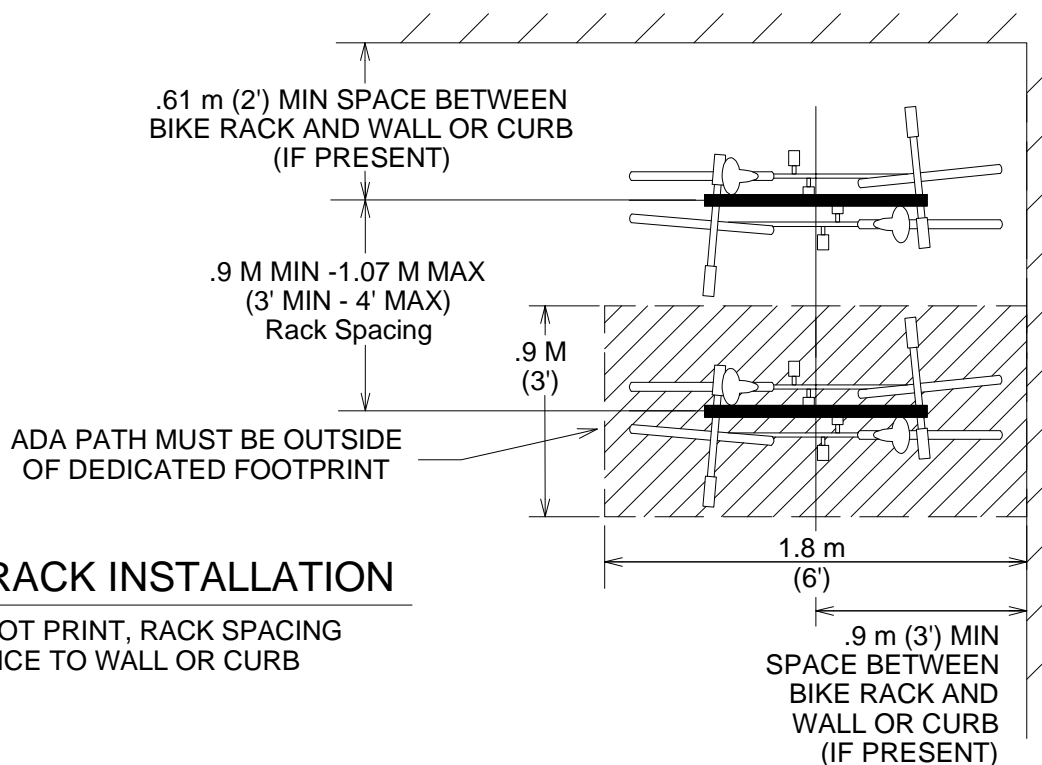
1. RACKS SHALL COMPLY WITH CITY STANDARD DETAIL 710S-1, TYPE III RACK FOR 1 TO 2 BIKES.
2. RACK PLACEMENT SHALL COMPLY WITH APPLICABLE CITY STANDARD DETAILS 710S-6A, 710S-6B, OR 710S-6C AND IN COMPLIANCE WITH SECTION 25-6-477 OF THE AUSTIN CITY CODE OR ITS SUCCESSOR.

CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS		BICYCLE RACK INSTALLATION IN SIDEWALKS-ALTERNATE 2	
RECORD COPY SIGNED BY COUNG TRAN	XX/XX/XX ADOPTED	THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	STANDARD NO. 710S-5 1 OF 1



LINEAR RACK INSTALLATION

DEDICATED FOOT PRINT, RACK SPACING
AND DISTANCE TO WALL OR CURB



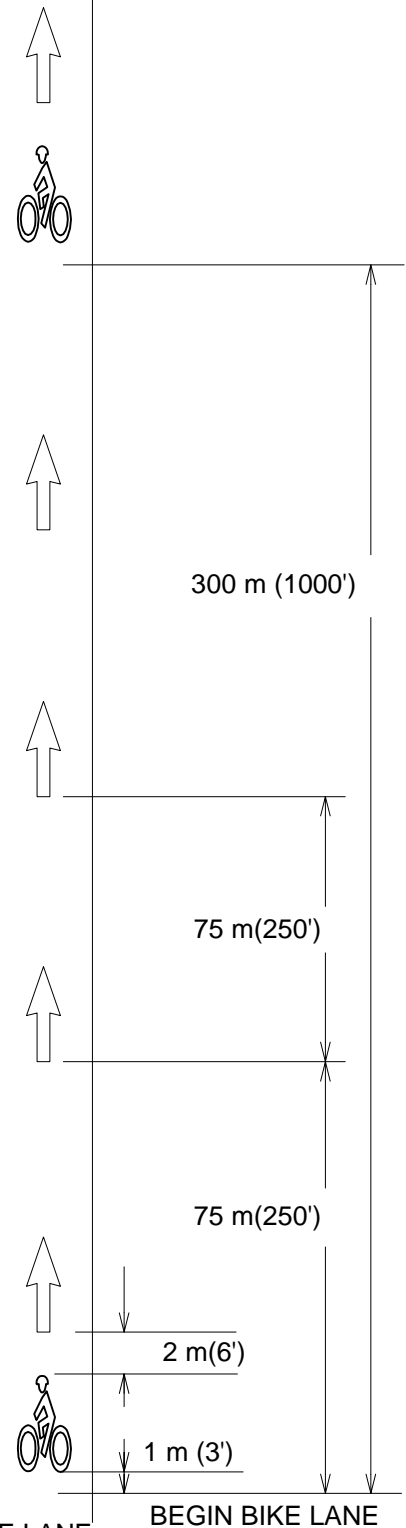
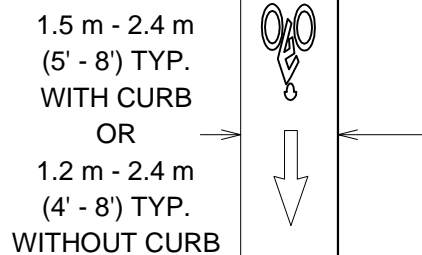
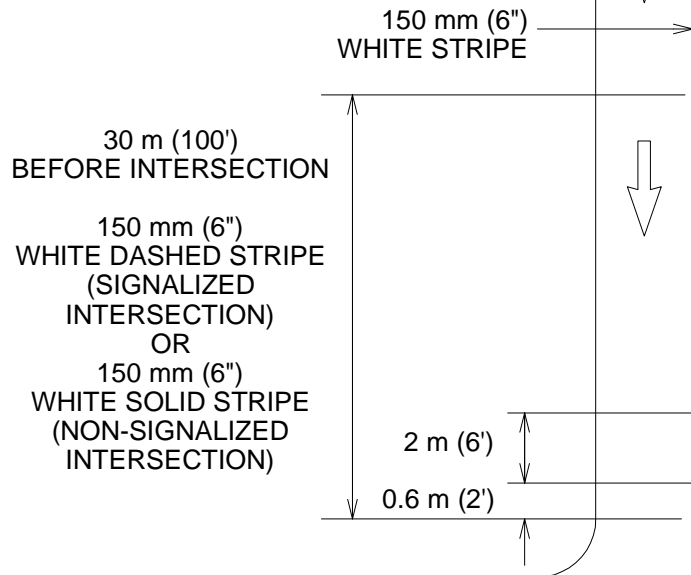
PARALELL RACK INSTALLATION

DEDICATED FOOT PRINT, RACK SPACING
AND DISTANCE TO WALL OR CURB

GENERAL NOTES:

1. RACK MUST COMPLY WITH CITY STANDARD DETAIL 710S-1, TYPE III RACK FOR 1 TO 2 BIKES.
2. RACK INSTALLATION MUST COMPLY WITH CITY STANDARD DETAIL 710S-3, 710S-4, OR 710S-5.
3. BIKE RACKS MUST BE PLACED IN COMPLIANCE WITH CITY OF AUSTIN CODE SECTION 25-6-477.
4. BIKE RACKS MUST BE LOCATED WITHIN 7.32 m (24') OF EITHER THE MAIN BUILDING ENTRY OR THE ENTRY TO THE PRIMARY LOCAL USE.
5. BIKE RACKS MUST BE PLACED PERPENDICULAR TO THE CURB WITH THE CENTERLINE OF THE RACK IN LINE WITH ANY ADJACENT TREES OR LIGHT POLES.
6. THE MINIMUM CLEARANCE BETWEEN BICYCLE RACK AND ANY PUBLIC OR PRIVATE UTILITY APPURTANANCE MUST BE 9m (3').
7. BIKE RACKS MAY NOT BE MOUNTED ON TOP OF VAULTS OR STORM DRAIN INLETS.

CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS		FURNISHING LOCATION IN 12' WIDE OR LESS SIDEWALKS - TYPE III BICYCLE RACK SITTING	
RECORD COPY SIGNED BY _____	XX/XX/XX _____ ADOPTED	THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	STANDARD NO. 710S-6C 1 OF 1



NOTES:

1. PLACE BIKE SYMBOL AND DIRECTIONAL ARROW AT START OF EVERY BIKE LANE.
2. PLACE BIKE SYMBOL AND DIRECTIONAL ARROW EVERY 300 m (1000') AND AFTER EACH INTERSECTION
3. PLACE DIRECTIONAL ARROW AT 75 m (250') INTERVALS BETWEEN BIKE SYMBOL/ARROW GROUPS.
4. LANE WIDTHS ARE MEASURED FROM CENTER OF BIKE LANE STRIPE TO THE FACE OF CURB OR EDGE OF PAVEMENT AT THE SHOULDER FOR A ROAD WITHOUT A CURB

CITY OF AUSTIN
DEPARTMENT OF PUBLIC WORKS

BICYCLE LANE MARKINGS

RECORD COPY SIGNED
BY XXXXXXXXX XXXXXXXXX

XX/XX/XX

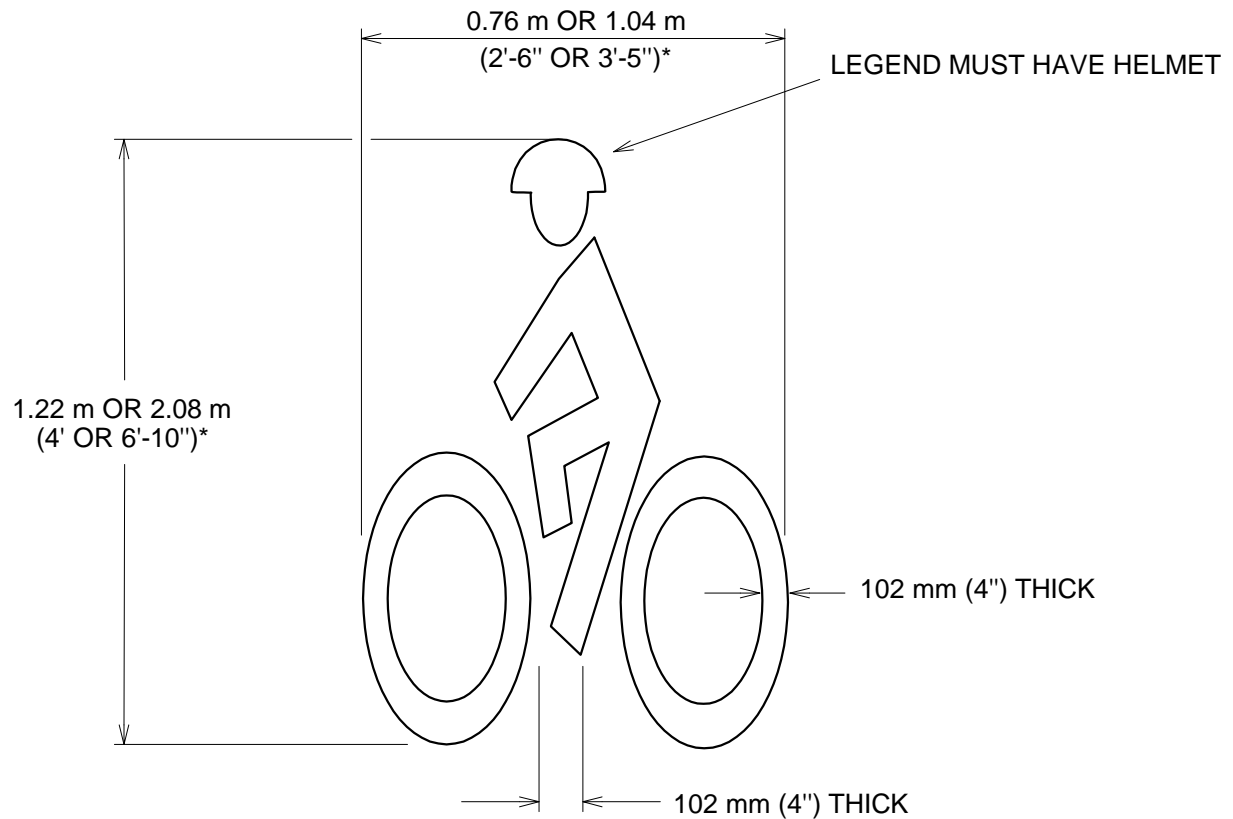
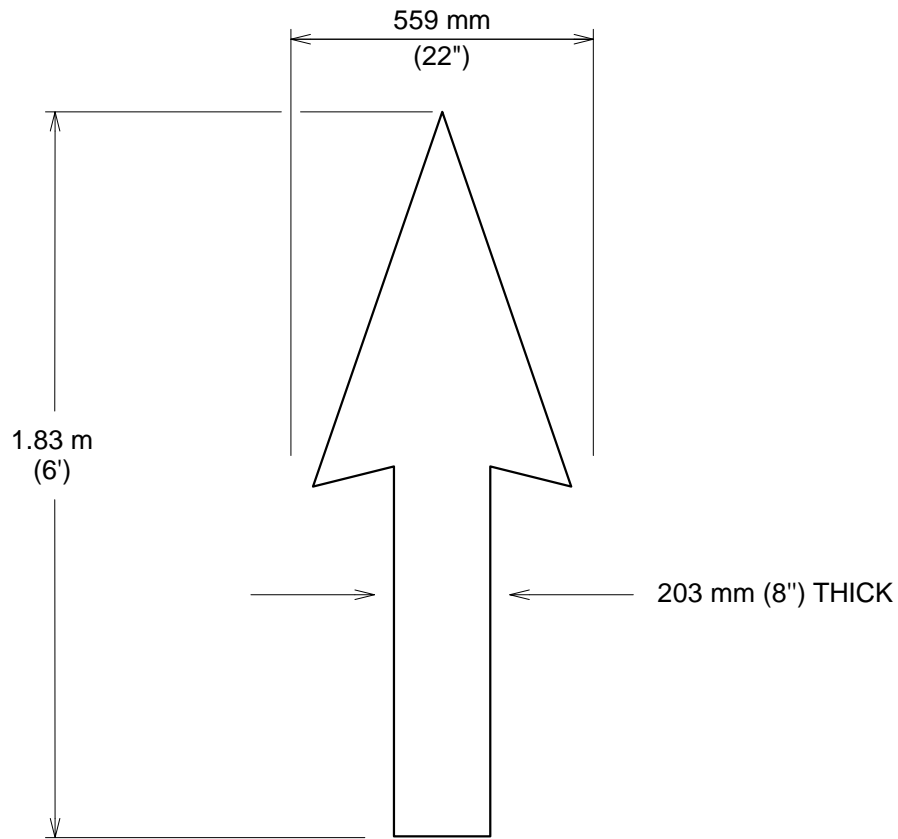
ADOPTED

THE ARCHITECT/ENGINEER ASSUMES
RESPONSIBILITY FOR APPROPRIATE USE
OF THIS STANDARD.

STANDARD NO.

829S-1A

1 OF 2



* DIMENSION IS DEPENDENT ON LANE WIDTH

CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS		BICYCLE LANE MARKINGS	
RECORD COPY SIGNED BY XXXXXX XXXXXXXX	XX/XX/XX	THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	STANDARD NO.
	ADOPTED		829S-1A 2 OF 2